

**SCMAGLEV Baltimore DC
LOOP PROJECT DC Baltimore
MS4 Update**

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**Agricultural Research Service
Beltsville Agriculture Research Center
Beltsville, Maryland**

BALTIMORE-WASHINGTON
SUPERCONDUCTING MAGLEV
PROJECT

Research Center

Illustrative Rendering of the Proposed MD 198 TMF, Looking West



**Illustrative Rendering of Proposed Alt. J Adjacent to BWP NB
Approaching MD 198, Looking North (arrow pointing at elevated viaduct)**



**Illustrative Rendering Aerial View of Proposed Alt. J Underground
Tunnel Portal Guideway Transition to Elevated Viaduct at
BARC Property Adjacent to BWP, Looking North**



**Illustrative Rendering Aerial View of Proposed Alt. J1 Underground
Tunnel Portal Guideway Transition to Elevated Viaduct at BARC Property
Adjacent to BWP, Looking North**



Illustrative Rendering of Proposed Alt. J1 Elevated Viaduct at Powder Mill Road



Washington, D.C. to
Baltimore Loop Project
Proposed by The Boring
Company

Potential Affects On BARC and
the USNA

The Loop System

- Enables high-speed travel: The Project offers high-speed passenger travel between Washington, D.C. and Baltimore, with anticipated travel time being approximately 15 minutes.
- Does not divide communities: Loop tunnels do not divide communities or public spaces with above-ground barriers or lanes.

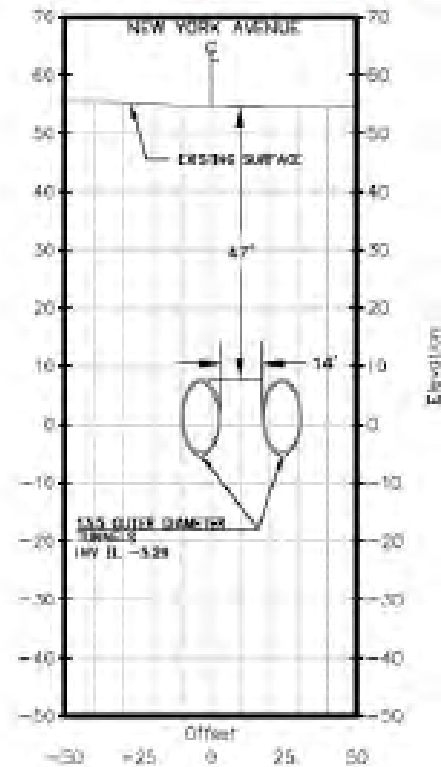
FIGURE 1-1: CONCEPTUAL RENDERING OF AN AEV INSIDE A MAIN ARTERY TUNNEL



FIGURE 1-2: LOCATION MAP OF THE PROPOSED PROJECT

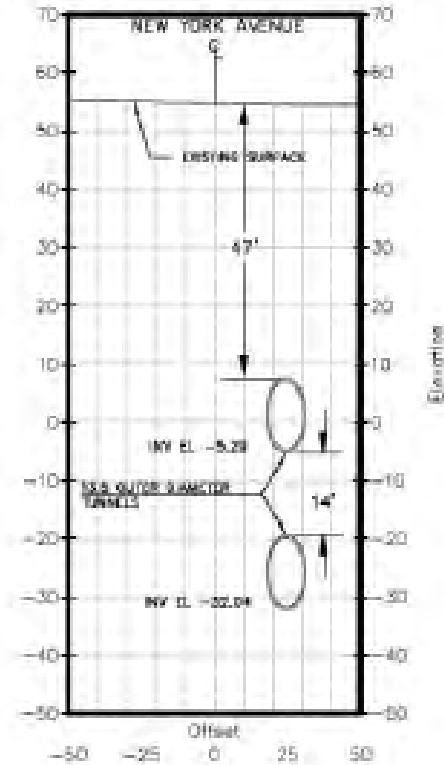


FIGURE 1-4: TYPICAL TUNNEL CROSS-SECTION SHOWING TWO POSSIBLE CONFIGURATIONS: PARALLEL (VIEW 1) AND STACKED (VIEW 2)



**TYPICAL SECTION
VIEW 1**

HORIZONTAL SCALE: 1"=40'
VERTICAL SCALE: 1"=20'



**TYPICAL SECTION
VIEW 2**

HORIZONTAL SCALE: 1"=40'
VERTICAL SCALE: 1"=20'

Note: Configuration can change along the length of the alignment.

LOOP STATIONS

333 Camden Street, Baltimore, MD. This public

FIGURE 2-1: CONCEPTUAL RENDERING OF TUNNEL LINING

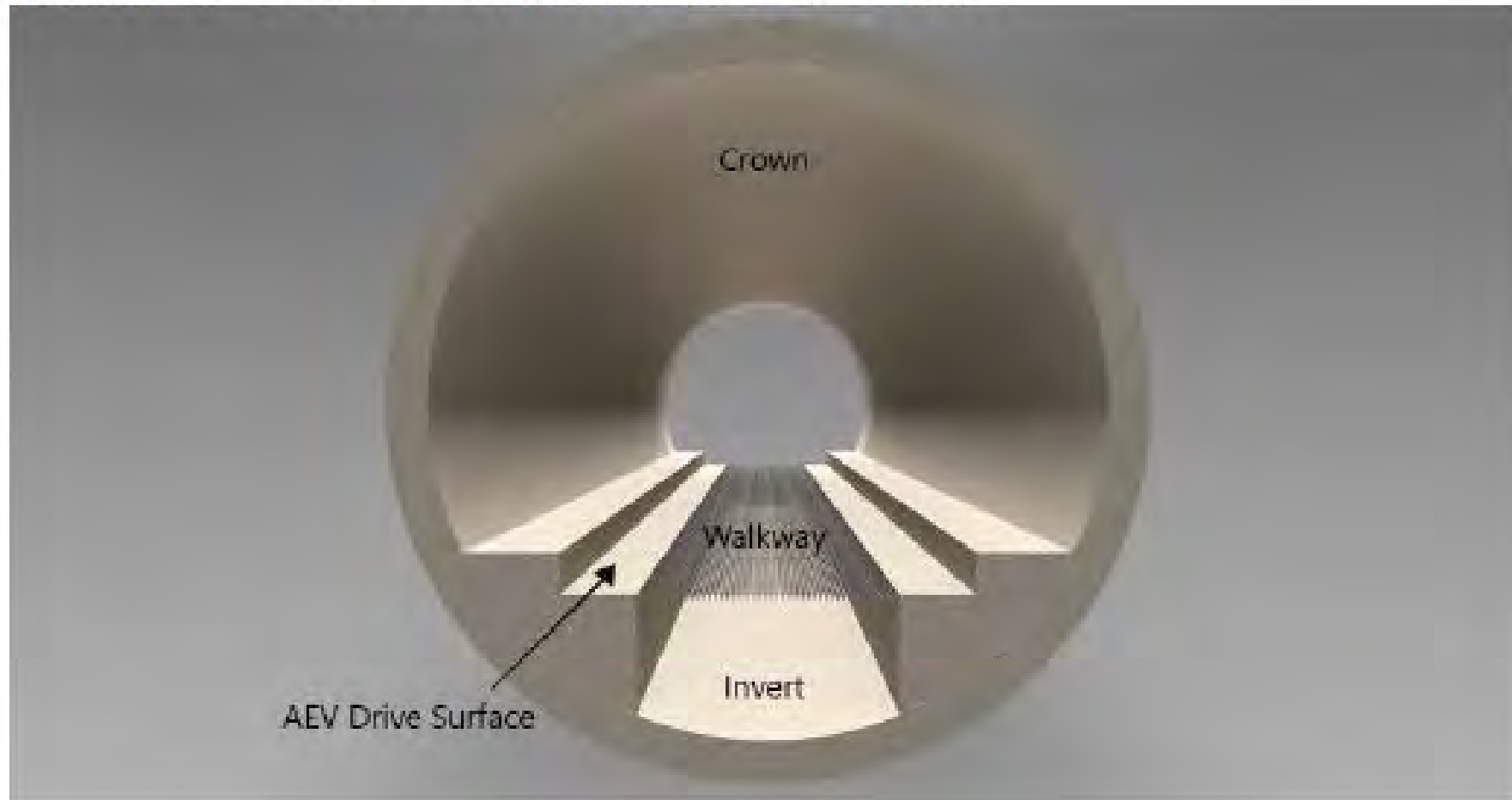
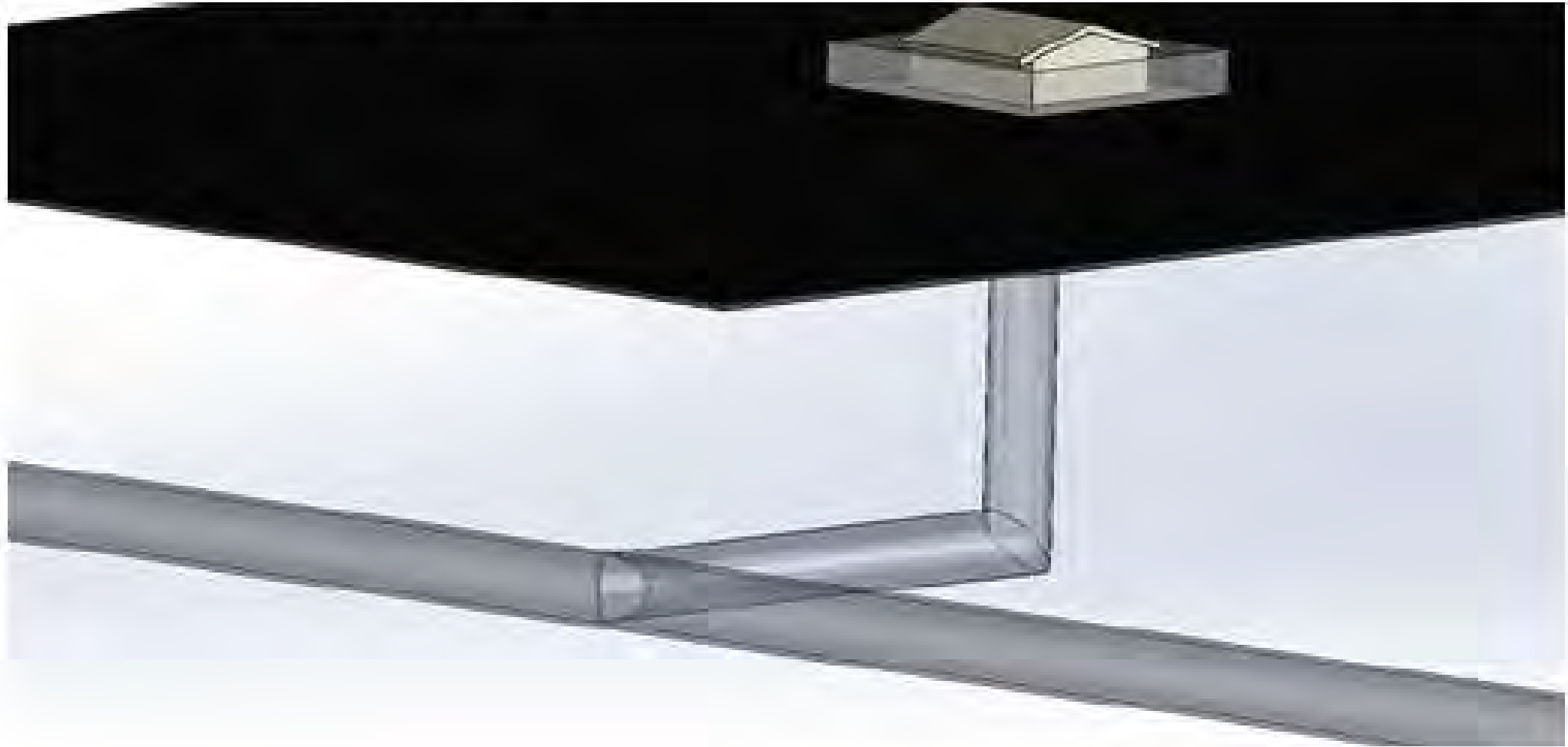


FIGURE 1-6: CONCEPTUAL ARTISTIC RENDERING OF VENTILATION SHAFT



FIGURE 2-2: RENDERING OF CONCEPTUAL VENTILATION SHAFT CONNECTION TO MAIN ARTERY TUNNEL



BARC

FIGURE 1-3: DETAILED PROPOSED PROJECT BOUNDARIES MAP (PAGE 16 OF 25)



Washington DC to Baltimore Loop Project
Project Boundaries Map - Page 16

FIGURE 1-3: DETAILED PROPOSED PROJECT BOUNDARIES MAP (PAGE 17 OF 25)



Washington DC to Baltimore Loop Project
Project Boundaries Map - Page 17

USNA



FIGURE 2-4: CONCEPTUAL RENDERING OF MAINTENANCE TERMINAL

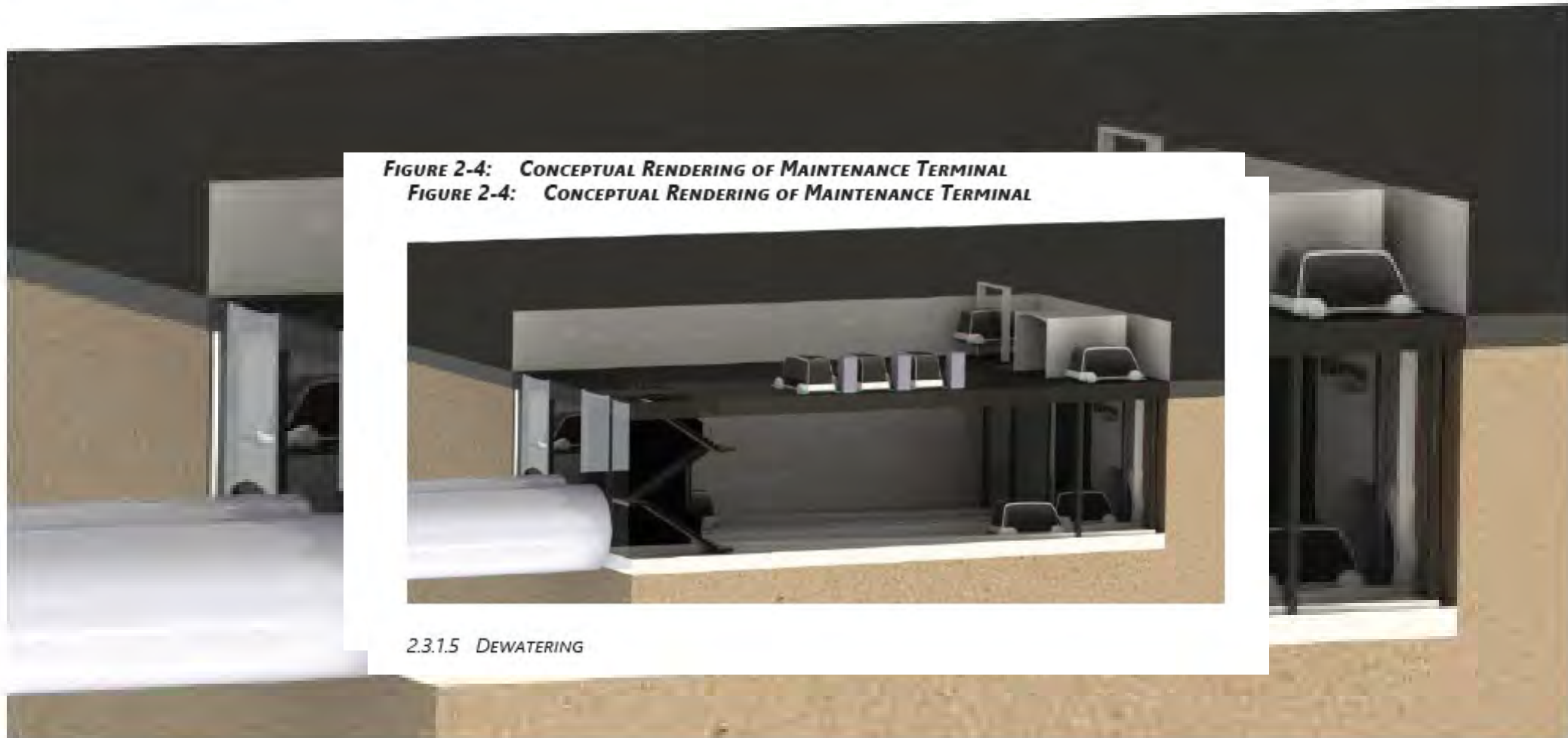


FIGURE 2-4: CONCEPTUAL RENDERING OF MAINTENANCE TERMINAL
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2.3.1.5 DEWATERING

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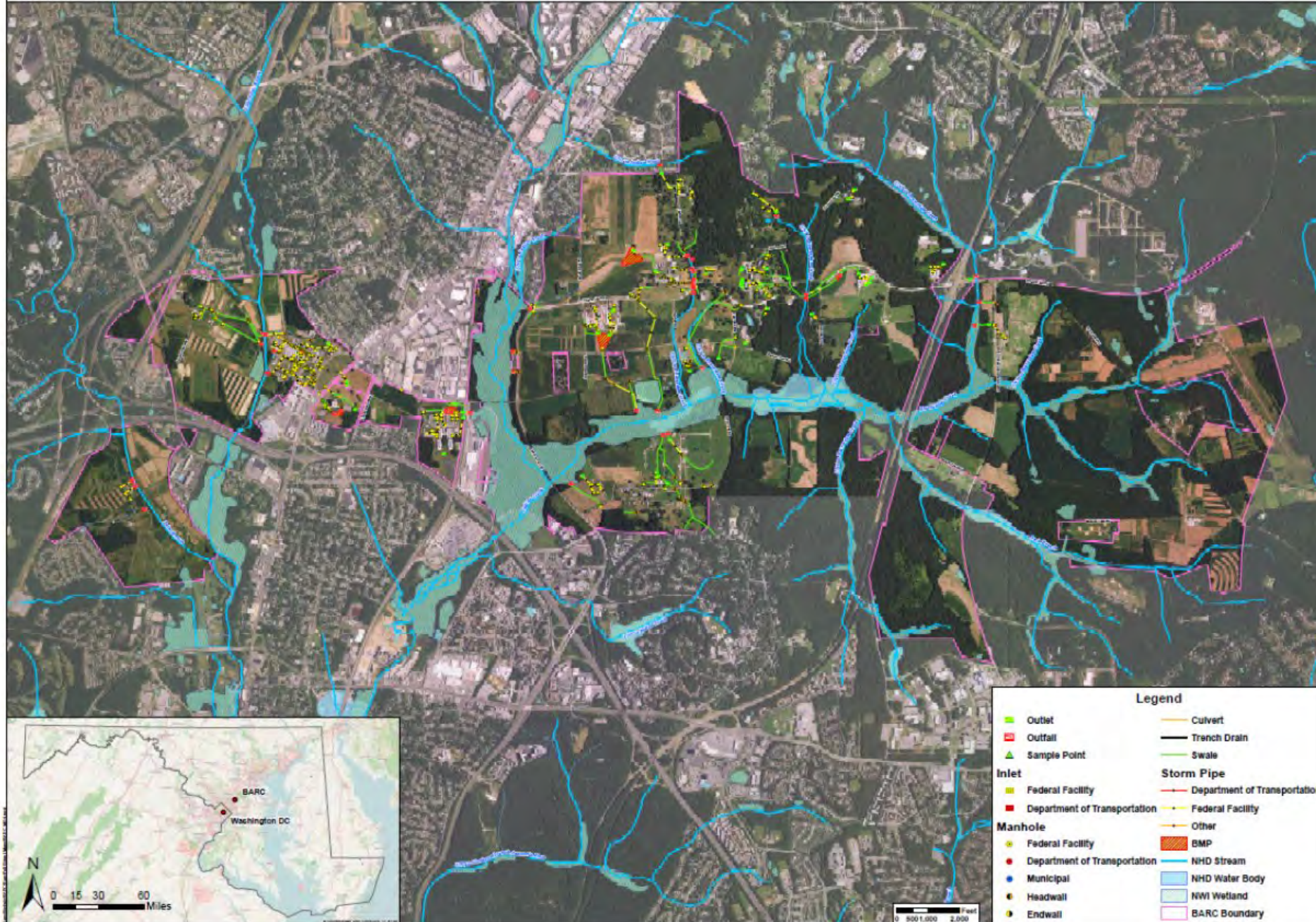
Build Alternative

- Based on the construction assumptions found in Appendix B, approximately 2,000,000 cubic yards of soil would be excavated over a projected tunnel construction period of 12 to 20 months.



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Stormwater Conveyance System



3 P Wetland Mitigation Project 39° 1'30.10"N 76°56'10.55"W



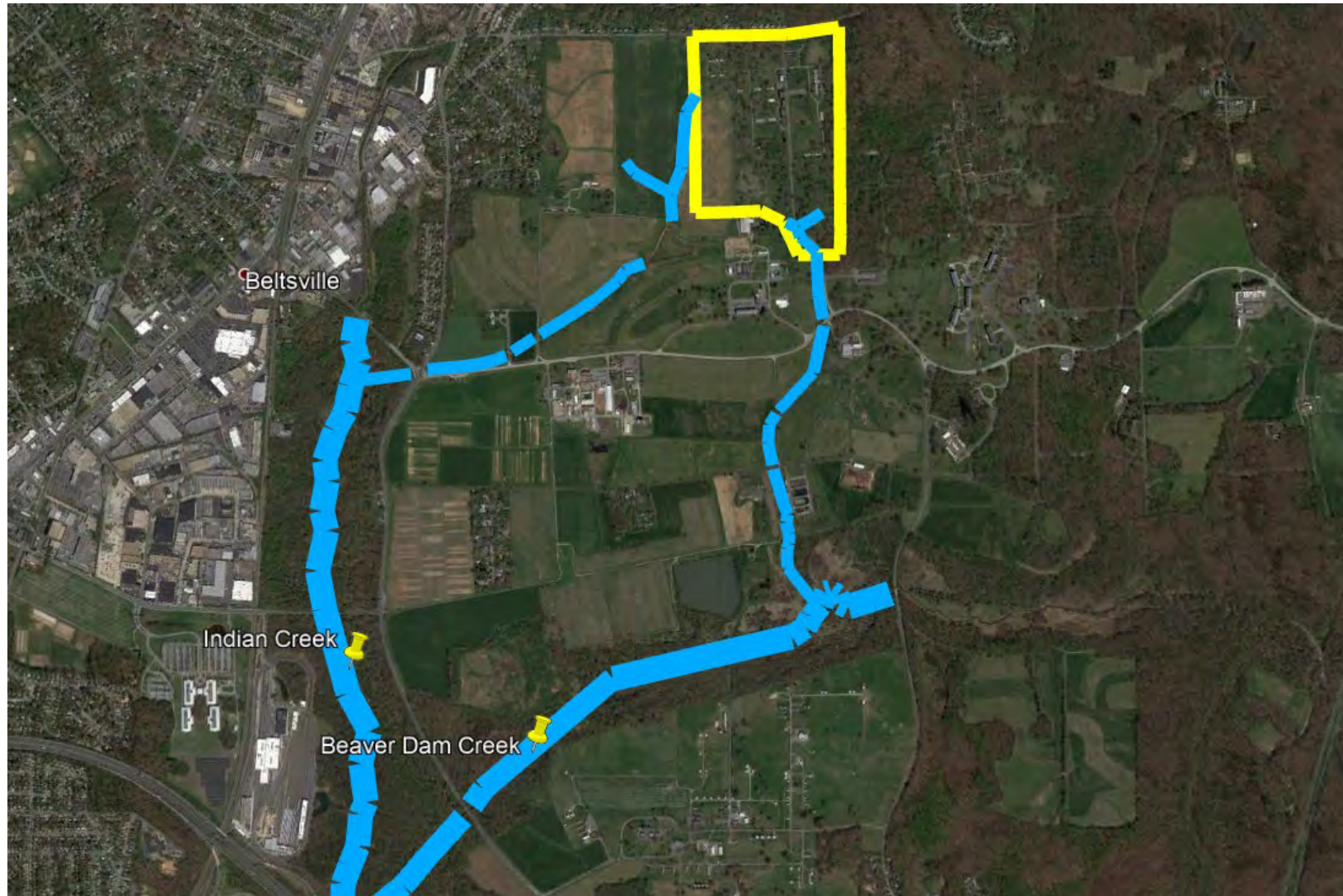
Field Tiles



Drain Tiles




MOA USDA and PG County



Thank You

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